

Committee Date	09/03/2022		Agenda Item:
Address	The Princess Royal University Hospital Farnborough Common Orpington BR6 8ND		
Application number	21/03190/FULL1	Officer Jessica Lai	
Ward	Farnborough and Crofton		
Proposal	Erection of a 197 spaces parking deck		
Applicant		Agent	
Kings College Hospital NHS Foundation Trust 9 York Place Leeds LS1 2DS		Mr Jonathan Dunbavin ID Planning 9 York Place Leeds LS1 2DS	
Reason for referral to committee	Major Application outside delegated authority; Planning enforcement	Councillor call in No	

RECOMMENDATION	<u>Planning permission be approved, subject to planning conditions</u>
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<p>KEY DESIGNATIONS</p> <ul style="list-style-type: none"> • Biggin Hill Safeguarding Area • London City Airport Safeguarding Area <p>Smoke control</p>

Vehicle parking	Existing number of spaces	Total proposed including spaces retained	Difference in spaces (+ or -)
Standard car spaces	212 (Surface level)	392 (195 spaces re-provision at surface level and 197 spaces at deck level)	+180 (or +36 for the wider hospital site)

Electric car charging points	80 electric vehicle charging point (40 active and 40 passive)
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Representation summary	The 21 days neighbour and planning consultee consultation letters were sent on the 10 th August 2021. A site notice was issued on the 10 th August 2021. The application was advertised in the News Shopper on the 18 th August 2021.
Total number of responses	
Number in support	1
Number of objections	3
Number of comment	1

1. SUMMARY OF KEY REASONS FOR RECOMMENDATION

The proposal is designed to meet the unmet parking provision associated to the approved development in 2015. The proposal is also designed to accommodate a future new endoscopy unit within the wider hospital site. The new endoscopy unit does not form part of this application. Disruption associated to the provision of car park deck will be minimised and an off-site staff car park with 130 spaces and shuttle bus services will be provided during the construction of the car park deck.

The proposal would result in a net increase of 36 parking spaces within the wider hospital site (or 180 spaces of the proposed site). Whilst the provision of additional parking spaces would not comply with the London Plan, the level of increase is considered acceptable at this location as the site has a moderate accessibility by public transport.

The siting, layout and design of the proposal is considered acceptable and would not have an adverse impact on the neighbours' residential amenities. Replacement planting including surface water flooding details will be provided and secured by planning conditions.

The construction period of car park deck should be minimised, where possible and the off-site staff car park including shuttle bus services should remained available for staff prior to the first use of the car park deck. Subject to the

planning conditions, it is considered that the proposal would be acceptable and is recommended that planning permission should be granted.

1. LOCATION

- 1.1 The Princess Royal University Hospital is surrounded by Welbrook Road to the east /south-east, Barkway Drive to the South /South-west and Starts Hill Road to the north. To the west of the hospital is a supermarket retail store (Sainsbury) with ancillary open air car park and a hospital staff car park. To the North, is Summercroft Surgery and houses on Starts Close. The hospital is surrounded by a mixture of residential and commercial uses.



Fig.1. Aerial photo.

- 1.2 The main vehicular access to the PRUH is via Farnborough Common (A21). The A21 forms part of the Transport for London Road Network (TLRN) where Transport for London (TfL) is the highway authority. The PRUH is also accessible via Starts Hill Road. There is a pedestrian access route via the adjacent Sainsbury car park, off Crofton Road.
- 1.3 The PRUH have a total of 536 existing car parking spaces in the hospital and an additional 108 staff parking spaces in Sainsbury car park, and vehicular access via Crofton Road.
- 1.4 The application site is an area of existing surface level car park containing 87 staff parking spaces and 125 shared staff and visitors parking spaces. The site is located opposite to the main hospital building, near the Accident and Emergency entrance and measures approximately 7, 677sq.m in area.
- 1.5 The Environment Agency's mapping record indicates that the PRUH is located within Flood Zone 1 and subject to very low risk of river flooding. The PRUH is also subject to low surface water flood risk.

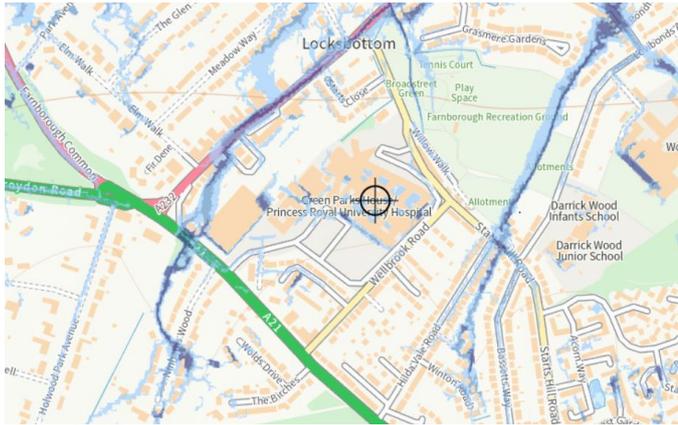


Fig 2. Surface water flooding

- 1.5 The public transport accessibility is rated at 2 and 3, on a scale between 0 to 6b where 0 is worst and 6b is excellent. There are two bus stops at the hospital and it is served by 3 bus routes (Route 261, R3 and R4). There are no trees served with Tree Preservation Orders within the site. There are no identified heritage assets within the site.

2. PROPOSAL

- 2.1 Full planning permission is sought for the provision of a car park deck to provide 197 staff parking spaces. The proposed car park deck would be located in an area of existing surface level car park, opposite to the main hospital building and near to the Accident and Emergency entrance door. The proposed car park deck would measure approximately 64 metres wide, 80 metres long with a deck height measures approximately 2.85 above the existing surface level at southern end and 3.5 metres at the north. The upper deck is 2.85 metres above the ground level with a further 1.25 high barrier inclusion of anti-glare panels.

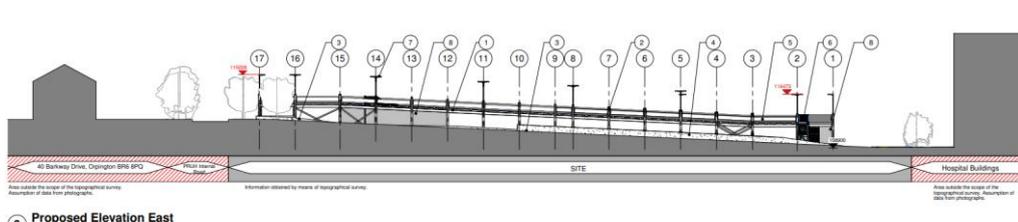


Fig 3. Proposed East elevation

- 2.2 The existing surface level car park comprises of a total of 212 parking spaces, of which 87 spaces are staff parking spaces and 125 spaces are shared staff and visitor parking spaces (shared spaces). As part of this proposal, the surface level car park would be reconfigured providing 195 shared parking spaces.



Fig 4. Existing surface layout parking layout (left)

Fig 5. Proposed surface/ground floor level layout (right)

2.2 The proposed car park deck would provide 197 staff parking spaces. The proposal would provide a net increase of 180 spaces (a net increase of 70 shared parking spaces at surface level and an additional 110 staff parking spaces at the deck/first floor level). The existing, proposed and difference of parking spaces and arrangement are tabled and outlined below:

	Existing	Proposed	Difference
Ground floor staff	87	0	-87
Ground floor shared	125	195	+70
First floor staff	N/A	197	+197
Total	212	392	+180

Table 1. Existing and proposed parking spaces



Fig 6 Proposed deck/first floor level layout

2.3 The proposed car park deck will be made of steel frame and concrete slabs. The deck will include a galvanised vehicle impact barrier and lattice mesh panels. The elevations facing the main hospital building

and houses on Barkway Drive would be installed with solid glare protection panels, attached on the vehicle impact barriers to restrict any light spillage.



Fig 6 and 7. Computer generated images

- 2.4 A total of 80 electric vehicle charging points would be installed, of which 40 charging points would be active and 40 charging points would be passive.
- 2.5 LED ceiling, wall mounted and floodlights would be installed. The floodlights would be made of aluminium and would be silver grey in colour. External lighting at the deck level would be all directed downwards to illuminate its target floor area. The specifications of the lighting are as follows:
- Glamox i40-1500 LED 5500 840 PC (569 lm);
 - Glamox i40-1500 LED 5500 840 PC (5691 lm); and,
 - Glamox O52-340 LED 4500 840 WBA (4350 lm).
- 2.6 An off-street staff car park with 130 parking spaces and shuttle bus services would be provided during the construction of the car park deck.

3. RELEVANT PLANNING HISTORY

3.1 21/05401/FULL1 - pending decision

Erection of part single storey/part two storey extension (north west corner) providing linking bridge/corridor to transfer patients.

3.2 21/05259/FULL1 – pending decision

Retention of 2x modular buildings (single storey Lung Function Unit and 2 storey Mental Health Assessment Unit). (Retrospective)

3.3 21/04639/FULL1 – pending decision

Erection of 2 storey substation containing a Low Voltage Transformer and Switch Room.

3.4 21/04644/ADV - Granted on the 22nd December 2021

Display of 3x 9m high non-illuminated NHS flagpole signs.

- 3.5 20/05022/FULL1 – granted on the 31st March 2021

Construction of single storey extension plus undercroft to northwest elevation of Princess Royal University Hospital to provide bed store.

- 3.6 20/03896/TELCOM – prior approval granted on the 15th December 2020
Upgrade of existing rooftop telecommunications equipment to include addition of 1no. EE and 1 no. H3G GPS modules installed on top of Aperture support poles, 2no. Apertures mounted on a tripod mounted on a new raised steel grillage, GRP panels/polycarbonate mesh screening, and associated ancillary works.

- 3.7 19/04726/FULL1 – pending decision

Erection of a 120 space parking deck and retention of 4 cabins

- 3.8 15/00842/FULL1 – granted on the 21st July 2015

Erection of: a two storey office building to the north-western elevation of the main hospital for a temporary period of 3 years; a single storey Critical Care Unit to the south-eastern elevation; removal of two existing structures and erection of a two storey extension to the south-western elevation to provide an Urgent Care Centre; a two storey Medical Records Distribution building to the north-eastern boundary with Starts Hill Road; and a two storey extension to the northern elevation to provide a Medical Records Storage facility and provision of additional 93 car parking spaces with alterations to landscaping PART RETROSPECTIVE.

Condition 10 of this planning permission states:

Of the 93 parking spaces hereby approved, 70 shall be allocated to staff by way of staff parking permits and six months after the completion of the new parking layout, the applicant will provide to the Local Planning Authority details of the number of additional staff parking permits issued. If this is less than the number of spaces provided in this permission they will also include details of proposals to increase the take up of the staff parking permits to be agreed by the Local Planning Authority.

Reason: In order to comply with Policy T3 of the Unitary Development Plan and in the interests of fully utilising the parking provision and reducing on street parking.

- 3.9 Various dated minor developments/applications not considered to be directly relevant to the current application.

4. CONSULATION SUMMARY

4.1 Highways

- Parking spaces

The planning application record indicates that the approved development in 2015 requires a total of 93 new parking spaces to be provided and the current application is designed to meet this unmet requirement. The transport assessment indicates that the applicant is planning to provide a new Endoscopy unit and this future provision would result in a future loss of 51 staff parking spaces within the wider hospital site, outside of the red line boundary of this application site.

The proposed car park deck would provide 197 staff parking spaces and in order to provide the proposed car park deck, the surface level car park will need to be realigned, resulting in a reduction of 17 existing surface level parking spaces. As a result, the proposal would provide a net increase of 180 spaces (197-17 spaces) at this surface car park location. It should be noted that when taking into account the required parking provision associated to the approved 2015 development and the future requirements of the new Endoscopy unit, the proposal would represent a net increase of 36 parking spaces at the hospital complex.

	No. of parking spaces	Increase /decrease in spaces
Existing hospital parking spaces	644 (536 spaces at hospital and 108 spaces at Sainsburys)	N/A
Proposed car park deck	197	+ 197
Realignment of existing surface car park and erection for a car park deck	212 (existing) 195 (proposed)	-17
New Endoscopy Unit (Separate application)	51	-51
Total hospital parking spaces	773	+129
The 2015 consent scheme	93 (required)	-93
Net increase (Meeting 2015 scheme and new endoscopy unit requirements)	N/A	+ 36 (773- 644 - 93)

Table 2. Changes of hospital parking spaces.

The use of spaces of the wider hospital site is tabled below:

	Existing	Proposed
Shared spaces	304	376
Blue Badge spaces	29	29
Staff spaces	203	260
Total on hospital site	536	665
Sainsburys (staff)	108	108
Total	644	773

Table 3. Existing and proposed parking spaces

The A21 forms part of the TRLN and TfL is the highway authority. TfL have raised concerns regarding the increased car usage and the potential to undermine bus usage. The proposal would potentially reduce the parking pressure on surrounding roads where people are already driving to the hospital and there are currently hospital visitors using the Sainsbury's car park.

There are no details regarding to the construction phase submitted with this application. The installation of this prefabricated car park deck would be very disruptive as a considerable number of existing spaces would not be available during the construction period. A construction management plan should include the remedial measures as a result of the works. Should planning permission be recommended, planning conditions requiring the car park deck be completed before the endoscopy unit is commenced is recommended.

The proposal would provide additional parking spaces for staff, officers would need to be confident that there will be an uptake of staff parking permits, and this can be pursued by condition similar to the 2015 application.

- Trip generation

The TA indicates that the proposed trips generated by the net increase in car parking over consented levels equates to 17-18 two-way trips onto the A21 and 6-8 two-way trips onto Starts Hill Rd during peak hours. The staff car parking accessed from Starts Hill Rd is linked with the main car park and so if all staff utilising the deck were to use the A21 the additional trips would be about 21-26 two-way trips during the peak hours. TfL raised no objection to these levels of trip generation.

Traffic counts were carried out at the hospital in November 2019 (pre Covid) which were used to estimate parking accumulation on site. The maximum parking accumulation was 91%. It obviously varies day to day as I have been to the hospital on more than one occasion where the

public parking on the site was over 100 percent. There is no alteration to the servicing arrangements.

The following conditions should be attached:

- Parking
- Construction management plan (including remedial measures due to loss of parking during the construction phase)
- Six months after the completion of the new parking layout, the applicant will provide to the Local Planning Authority details of the number of additional staff parking permits issued. If this is less than the number of spaces provided in this permission, they will also include details of proposals to increase the take up of the staff parking permits to be agreed by the Local Planning Authority. Reason: In the interests of fully utilising the parking provision and reducing on street parking

4.2 Transport for London

Whilst there is a slight reduction in parking spaces, the justification for the expansion of car park remains unclear particularly without a planning application for the endoscopy unit. This is contrary to the London Plan and Mayor's Strategy policies that seek to encourage mode shift to active travel and public transport, reduce congestion and carbon emissions, improve air quality and reduced accident risk in line with vision zero.

The Transport Assessment (revision 4 dated December 2021) suggests that the car park is at 90 percent peak occupancy. If the new on-site car parking is proposed to prevent on-street car parking in the surrounding residential roads, without expansion of residential permit-only parking there is an obvious risk that the additional on-site car parking will not solve the problem of the on-street parking problem in the neighbouring roads. The principle of this development would be more acceptable when linked to the new endoscopy development/planning permission, which would presumably increase trips and of course remove some existing car parking. For example, a phased opening of the new car park, with part of/one deck of the car park closed off until the endoscopy unit is under construction (and existing car parking loss), whence the 'lost' 51 spaces are released, and then all spaces released when the endoscopy unit is operational, coupled with an expansion of the residents permit-only parking on the surrounding streets.

As the number of parking spaces is reduced and below 200, the LPA is unlikely to refer the application back to the GLA for Stage 2 consultation. If this is the case then TfL becomes a statutory consultee primarily in respect of impacts on the Transport for London Road Network (TLRN) and TfL public transport services. The proposal would introduce additional traffic in and out of the hospital and would exacerbate existing congestion on the TLRN, and increase accident risk,

simply as more vehicles would use the access junction and TLRN, but it would be difficult to demonstrate a severe adverse impact, as per the NPPF. Increasing car parking will undermine bus use also, at a time when passenger numbers are suppressed due to the pandemic, which could result in future service cuts, and increased congestion will add to bus journey times. On these issues, the Council as the sole planning authority will clearly need to decide the weight given to them in the decision-making process.

4.3 Drainage - lead local flood authority

Taking in consideration the fact that the constraint of the site in incorporating SUDS to attenuate for surface water run-off generated by the new deck. The applicant is requested to consider using decking area/part area to allow for some ponding up to 150mm during big events to ease some pressure on the drainage network downstream.

It is important to highlight that the drainage system is overwhelmed in areas such as Crofton Road where flooding is reported in many occasions.

An overlay drawing which to show the proposed site surface water drainage are connected into existing drainage system. It should be demonstrated that the pump discharge of 4.361l/s will be maintained for the proposed deck.

4.4 Thames Water

Waste water

The proposal is located withing 15 metres of Thames water underground assets and there are public sewers crossing or close to the development. An informative should be attached and the developers should read the guidance relates to working near to Thames Water assets or diverting Thames Water pipes for information.

Surface water drainage

Thames Water would advise that if the developer follows the sequential approach to the disposal of surface water we would have no objection. Management of surface water from new developments should follow Policy SI 13 Sustainable drainage of the London Plan 2021. Where the developer proposes to discharge to a public sewer, prior approval from Thames Water Developer Services will be required.

Developers are expected to demonstrate what measures will be undertaken to minimise groundwater discharges into the public sewer. A permit from Thames Water will be required for any discharge of ground water into a public sewer. Groundwater discharges typically result from construction site dewatering, deep excavations, basement infiltration,

borehole installation, testing and site remediation. Any discharge made without a permit is deemed illegal and may result in prosecution under the provisions of the Water Industry Act 1991. Any discharge made without a permit is deemed illegal and may result in prosecution under the provisions of the Water Industry Act 1991. An informative advising this should be attached.

Waste water network and sewage treatment works infrastructure capacity

No objection based on the information provided.

Water Comments

No objection based on the information provided. An informative advising Thames Water will aim to provide customers with a minimum pressure of 10m head (approx 1 bar) and a flow rate of 9 litres/minute at the point where it leaves Thames Waters pipes should be attached.

There are water mains crossing or close to the development Thames water do not permit any building over or construction within 3 metres of water mains and development is advised to read the guidance relates to working near or divert Thames water pipes.

4.5 Greater London Authority

The GLA was consulted in line with the Town and Country Planning (Mayor of London Order) 2008. The revised proposal would provide 197 spaces and is no longer considered to be a potential strategic importance application as defined by the Town and Country Planning (Mayor of London Order) 2008.

The GLA and TfL have been informed regarding to this updated proposal on the 17th February 2022 and the Council's intention to determine this application on the 9th March 2022, in line with the Council's scheme of delegation. The following planning consultation response can therefore be treated as a non-statutory response only:

Land use principle: The applicant must provide robust justification to demonstrate the additional car parking spaces are necessary for visitors, patient and operational needs of the hospital staff, and that appropriate parking controls would be secured to prevent displacement car parking on local surrounding streets.

Urban Design: The architectural expression of the elevations fronting the hospital and existing bus tops should be softened to enhance the streetscape and mitigate the visual impact of the extensive car parking. Detail consideration is also required on the location of the stair cores and impacts on public realm, legibility and pedestrian permeability. Step-free access must also be provided to the upper deck.

Transport: The additional on-site car parking occupied with no new on-street parking controls would undermine Mayoral mode shift targets for out London, discourage sustainable and active travel and increase accident risk.

Environment: The applicant must detail and committed to the compensation required in terms of substitute planting for the 62 trees to be removed and additional measures should be provided to improve the urban greening factor.

Sustainability: Further information is required in relation to energy and whole life-cycle carbon.

Officers comment:

The current proposal is no longer considered as a potential strategically important application under the Town and Country Planning (Mayor of London) 2008 Act. The Stage 1 GLA comment above is therefore, can be considered as a non-statutory planning consultation comment.

b) Local groups

None received.

c) Adjoining Occupiers

4.6 Three (3) letter of objections have been received and the grounds are summarised as follow:

1. Loss of outlook
2. Design and appearance
 - The proposal should be located on the other side of the hospital as the site was planned to increase additional hospital accommodation.
 - The existing layout and appearance of the hospital should be kept.
 - The proposed car park deck is unsightly and there are adequate parking spaces at the hospital.
3. Highways and parking provision
 - The proposal would encourage the use of car to the already heavily trafficked roads.
 - The proposal would not assist to reduce the parking pressure on the neighbouring road as there are no parking restrictions on Welbrook Road.
 - The additional spaces are for staff and should be located elsewhere, on the other side of the hospital as the site was planned to increase additional hospital accommodation.

- The car park deck should be erected in the Sainsbury car park which is occupied by the hospital staff.

4.7 One (1) letter of comment have been received and the grounds are summarised as below:

1. The proposal would address the inadequate parking provision at the hospital site and reduce the parking pressure on the neighbouring roads. Whilst the proposal could be similar to the parking deck at Orpington Station, it is not considered to be unsightly and would meet the needs of the end users.
2. Underground parking would be costly to build, and objectors should consider the timing the disruption it would cost for underground parking. The proposal would have less disruption than the suggested underground parking option.

4.8 One (1) letter of support have been received and the grounds are summarised as below:

1. The proposal would positively address the existing inadequate parking provision
 - Despite the hospital being accessible by bus, parking provision is inadequate particularly during peak hours on weekdays
 - Visitors and patients arriving for treatment or requiring the emergency department do not require more stress to park at/near the hospital.
 - Insufficient parking for staff who paid for monthly pass.
 - The proposal would reduce the parking demand on the neighbouring roads and reduce pollution with private motorists searching for parking spaces.
2. The old hospital was much more unsightly than the modest car park deck.
3. The houses on Wellbrook Road are screened by trees.

5. POLICIES AND GUIDANCE

5.1 National Planning Policy Framework 2021

5.2 National Planning Practice Guidance

5.3 The London Plan

Policy D3	Optimising site capacity through the design-led approach
Policy D4	Delivery good design
Policy D5	Inclusive design
Policy D11	Safety, security and resilience to emergency
Policy D12	Fire safety
Policy D14	Noise
Policy G5	Urban Greening
Policy G6	Biodiversity and access to nature
Policy G7	Trees and woodlands

Policy SI-1	Improving air quality
Policy SI-2	Minimising greenhouse gas emissions
Policy SI-13	Sustainable drainage
Policy T4	Assessing and mitigating transport impacts
Policy T5	Cycling
Policy T6	Car parking
Policy T6.5	Non-residential disabled persons parking
Policy T7	Deliveries, servicing and construction

5.4 Mayor Supplementary Guidance

- Accessible London: Achieving an inclusive environment 2011
- Character and context SPG 2014
- Energy Assessment Guidance 2020
- The control of dust and emissions in construction SPG

5.5 Bromley Local Plan 2019

Policy 30	Parking
Policy 31	Relieving congestion
Policy 32	Road Safety
Policy 33	Access for all
Policy 37	General design of development
Policy 73	Development and trees
Policy 79	Biodiversity and access to nature
Policy 115	Reducing flood risk
Policy 116	Sustainable Urban Drainage systems
Policy 119	Noise Pollution
Policy 120	Air Quality
Policy 122	Light pollution
Policy 123	Sustainable design and construction
Policy 124	Carbon dioxide reduction, decentralised energy networks and renewable energy
Policy 125	Delivery and implementation of the Local Plan

6. Assessment

6.1 Principle of development – Acceptable

6.1.1 In July 2015, a full planning permission was granted (planning reference: 15/00842/FULL1; dated 21st July 2015) to provide new and improved services and facilities at the hospital site with 93 additional parking spaces. Following the receipt of complaints from the members of the public regarding to the non-provision of 93 parking spaces, A planning breach of condition notice was issued by the Council's planning enforcement division in December 2017.

6.1.2 The location of the required parking spaces associated to the 2015 development was designed to be scattered around the hospital site as follows:

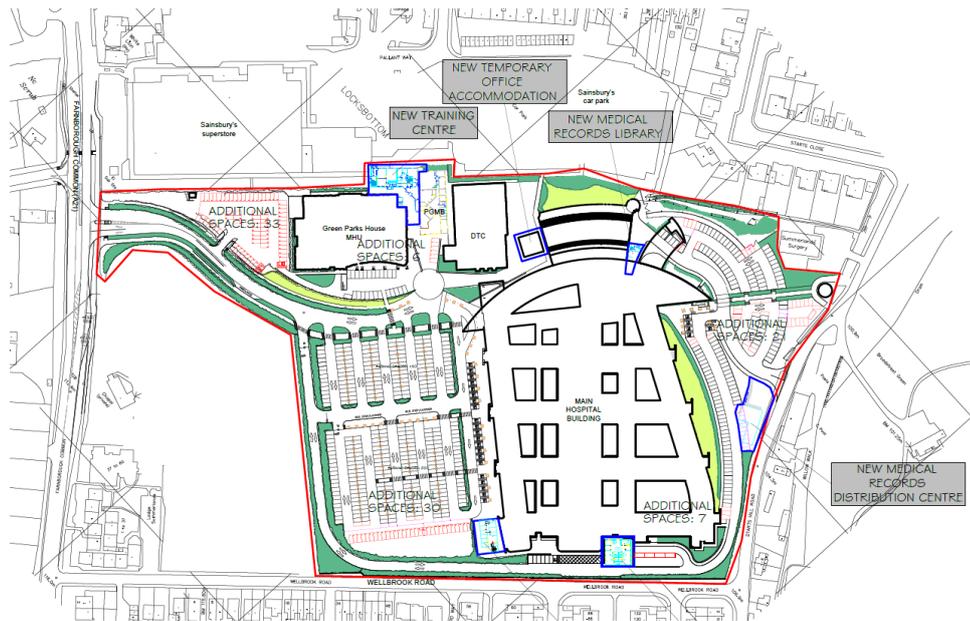


Fig 8. Approved parking layout in 2015

6.1.3 The Princess Royal University Hospital (PRUH) forms part of the King's College Hospital NHS Foundation Trust providing a wide range of valuable and essential health services in Bromley. The Trust is also planning to introduce a new endoscopy unit within the PRUH hospital complex, and 51 existing staff parking spaces within the wider hospital site will be removed to accommodate this future provision. The new endoscopy unit and the removal of the 51 parking spaces does not fall within the boundary of this planning application. This element will be subjected to a future planning application which is being registered, at the time of writing this report.

6.1.4 The current proposal is designed to meet the unmet parking requirement associated to the 2015 development and to ensure adequate parking spaces can be provided to support the operational needs of the existing and future users. It is noted that additional parking spaces would not be compatible to the London Plan objectives which seeks to promote sustainable modes of transport. Given the purpose of this proposal which is to address the previous unmet provision and the below average public transport accessibility rating, it is considered that the principle of the development would not be unacceptable, subject to:

- (1) the full details of parking mitigation measures during construction of the car park deck: and,
- (2) the construction works of the car park deck should be commenced prior to other development.

6.1.5 The impact on residential amenities, highways and sustainability are outlined in the following section of the report

6.2 Design – Acceptable

- 6.2.1 The proposed car park deck would be situated at the south-eastern corner of the hospital site, opposite to the main hospital building and near to the accident and emergency entrance. The existing surface level car park contains 87 staff parking spaces and 125 shared staff and visitor's parking spaces. The existing surface level car park will be realigned to accommodate the proposed car park deck. As a result of the realignment, the number of surface level parking spaces would be reduced from 212 to 195 spaces and these spaces would be provided as shared staff and visitors' parking spaces.
- 6.2.2 Objections have been received stating alternative locations should be sought for the proposed staff parking spaces. The location of the proposal is designed to enable additional parking spaces and more shared staff and visitors parking spaces can be located near to the main hospital building and doors. The proposed location is considered to be more beneficial for all users when compared with other locations, such as the supermarket car park via Crofton Road with pedestrian access via ramp. It is considered that the proposed location is acceptable.
- 6.2.3 The design and appearance of the proposed car park deck is contemporary and would be mainly made of steel and concrete. The construction of the car park deck would be mainly off-site and assembled on site. The construction time required for a car park deck would be around 4 to 5 months and is considered to be less disruptive than the other typical building with deep foundations which requires construction periods around 18 to 24 months. It is considered that the proposal would ensure the delivery of the additional parking spaces in a more manageable and efficient manner, making more efficient use of the existing hospital car park to meet the needs of the existing users.
- 6.2.4 The layout of the proposed car park deck has been designed to follow a clockwise, one-way circulation flow. This would offer drivers good opportunity to locate available parking spaces in a more organised manner and reducing the risk of collision. Pedestrian walkways and lighting would be clearly laid out including floodlights which would be installed.
- 6.2.5 The proposed car park deck would measure approximately 80 metres long, 64 metres wide, with a deck height which measures 2.85 metres above the existing surface level at the southern end and 3.5 metres at the north.
- 6.2.6 The north and south elevations facing the hospital building and the houses of Barkway Drive would be installed with a solid panel to avoid light spillage. The layout and design of the proposal is designed to minimise glare onto the residential houses on Barkway Drive and the hospital ward. The ramp would be constructed with concrete with vehicle impact barriers. This would minimise any travelling noise generated by

car. An example of the ramp and vehicle impact barriers are attached below:



Fig 9 and Fig 10. Example of car park deck

Secured by design

- 6.2.7 The proposal is designed to achieve Design Commercial 2015 guidance and Park Mark standard. The Secured by design officer was consulted and they considered that the proposal can achieve Secured by Design. A planning condition requiring secured by design accreditation is recommended and will be attached.
- 6.2.8 The applicant has advised that a temporary staff car park with shuttle bus services will be provided for staff during the construction period. The park-and-ride car park is located adjacent to the Dobbie Garden Centre on Oakley Road. It is considered that the proposed measures would assist to mitigate any temporary disruption and inconvenience which arises during the construction of the car park deck.

Fire Safety

- 6.2.9 The matter of fire safety compliance is covered by Part B of the Building Regulations. However, to ensure that development proposals achieve highest standards of fire safety, reducing risk to life, minimising the risk of fire spread, and providing suitable and convenient means of escape which all building users can have confidence in, applicants should consider issues of fire safety before building control application stage, taking into account the diversity of and likely behaviour of the population as a whole (London Plan Policy D12). A fire strategy report has been submitted. The proposal is an open deck car park. The means of warning, evacuation regime, emergency lighting, signage, access and facilities for the fire services and the provision of fire hydrants is identified.
- 6.2.10 Compliance to the fire statement will be conditioned however, compliance with the Building Regulations will still be required at the appropriate stage of the development.

6.3 Neighbourhood Amenity - Acceptable

- 6.3.1 BLP Policy 37 requires development will be expected to respect the amenity of the occupants of the neighbouring buildings and ensure that

they are not harmed by noise and disturbance, inadequate daylight, sunlight privacy or by overshadowing. Objections have been received relating to the impact on residential properties near the hospital. The houses on Barkway Drive are adjoining to the hospital site and would be mostly affected by the proposal. The residential properties and Wellbrook Road and surrounding the site may also experience a degree of impact.

Houses on Barway Drive

6.3.2 The two storey houses on Barway Drive are located to the south of the hospital existing car park. The ground level of Barway Drive is higher than the main hospital which drops down from Barkway Drive towards the main hospital building. The houses and the existing car park are separated by an existing access road within the hospital complex. There is also an avenue of tree between the existing access road and the rear gardens of Barkway Drive. The existing car park is installed with external lighting.

- Outlook

6.3.3 The proposal would be situated within the existing car park and there is no change in terms of its land use. The proposed car park deck would be positioned approximately 23 metres away from the back wall of the houses on Barkway Drive. It is noted that part of the proposed car park deck would be visible between the mature/semi-mature tree canopies. Given that a good distance between the proposal would be maintained and the proposal would remain screened by the existing trees along the southern boundary of the hospital site and the access road, it is considered that the proposals impact upon the neighbours' residential amenities in terms of outlook would not be significant.

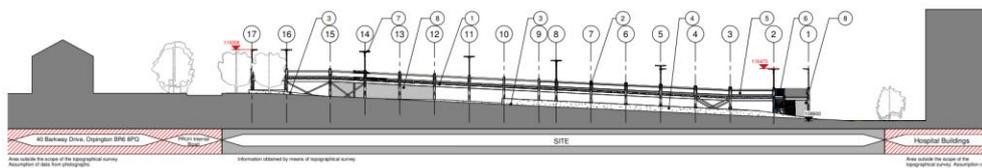


Fig 11. Proposed east elevation

- Light

6.3.4 The site is situated in a built-up area. There are external lights in the existing car park ancillary to the hospital buildings with an accident and emergency department in operation on a 24-hour daily basis. External LED lighting would be provided in the form of floodlights, approximately 5 metres in height from the deck level and ceiling or wall light mounted to the proposed car park deck. The external lighting is considered to be essential and is designed to be concentrated lighting onto the main car park area.

6.3.5 A lighting assessment is submitted which indicates the proposal would not have an adverse impact on the residential amenities of adjoining or nearby houses. Vehicle impact barrier and lattice mesh panels will be installed on the north and south elevation of the proposed deck. This would reduce and avoid light spillage from vehicles towards the houses on Barway Drive and the hospital wards. The Council's environment health has raised no objection to the proposal. As such, it is considered that the proposed external lighting would be acceptable.

- Noise

6.3.6 An environmental noise survey and assessment report is submitted which indicates the ambience noise level associated to the proposal would likely to be increased by less than 1dB (0.9 dB). This level is not discernible and is not expected to result in long term adverse effects.

6.3.7 The Council Environment Health was consulted, and no objection was received. It is considered that the proposal would not have an adverse impact on the neighbour's amenities.

- Air quality

6.3.8 The site is within Bromley Air Quality Management Area which means the site is located within an area that has been declared for exceedance of the annual mean Air Quality Objectives for nitrogen dioxide (NO₂). An air quality assessment is submitted which assess the likely effects of the proposals for the proposed end-users, and to assess potential impacts as a result of the development. The assessment indicates that proposal would not exceed the legal limits at operational phase. An air quality assessment is submitted which indicates that the potential impact on air quality is not considered to be significant, provided that there is a good dust control measures and prices in place.

6.3.9 Dispersion modelling was undertaken to quantify annual mean Nitrogen dioxide (NO₂), particulate matter (PM₁₀) and fine particles PM_{2.5} concentrations at sensitive receptor locations. This has been verified using local monitoring data. The dispersion modelling results indicated that predicted impacts on annual mean NO₂, PM₁₀ and PM_{2.5} concentrations as a result of operational phase exhaust emissions were predicted to be negligible at all 17 sensitive receptor locations within the vicinity of the site. The overall significance of potential impacts was considered to be not significant in accordance with the environmental protection and institution of Air quality management guidance. Pollutant emissions associated with the developments anticipated annual traffic movements were compared against the development specific transport emissions benchmarks (TEB). The result indicates that the Total Transport Emissions were below the development specific TEBs.

6.3.10 The Council's environmental health was consulted, no objection was received. As part of the proposal, a total of 80 electric vehicle charging

points would be installed and this would be secured by a planning condition.

Other residential properties

6.3.11 The residential houses on Welbrook Road are located approximately 34 metres east of the site. The houses are separated and screened by an internal access road, planting within the hospital site and Wellbrook Road. Due to the distance and its relationship, it is considered that the impact on residential amenities would not be unacceptable.

6.4 Transport - Acceptable

Access

6.4.1 The main vehicular access to the hospital car park and the proposed car park deck would be same as the existing arrangement, via the A21. The existing surface level car park would be realigned, and the car park deck would be accessed via a one-way system. The layout of the parking spaces would be clearly and well laid out. The Council's highway division have raised no objection to its siting, location and access arrangements.

Parking standards

6.4.2 Transport for London have raised concerns regarding to the additional number of parking spaces. TfL considered that the proposal would not comply with the London Plan.

6.4.3 There are no specific parking standards for hospitals. In considering the acceptability of the proposal, consideration should also be given to the use and need for additional parking spaces, location and accessibility of the site, existing and parking conditions of the hospital car park and surrounding area, the uptake of staff parking permits, planning application history of the site and the need to sustain the hospital operation and development.

6.4.4 The PRUH has a moderate public transport accessibility, rated at 2 to 3. A transport assessment including a traffic survey was carried out which indicates the hospital car park is at least at 91 percent occupation. Officers have visited the site on two different occasions and the car park was saturated with no or little space capacity on both occasions. The surrounding roads are also lined up with parked cars. The Council's highway officer has also advised that the availability of parking spaces was also close to saturated.

6.4.5 The introduction of more parking spaces would attract more vehicles parked at the hospital site. However, it should be noted that the 93 additional parking spaces was required and associated to the 2015 development. The overall net increase of parking spaces would be 36 spaces as a result to meet the identified unmet demand, realignment of

existing surface car park and the introduction of a new endoscopy unit. The net increase of the overall parking spaces within the hospital complex is therefore, considered to be insignificant and considered to be acceptable to support all users.

Staff parking permits

6.4.6 The Transport statement states that there are three types of staff parking permit / rights at the PRUH:

- Gold permit holders – covering all KCH Trust sites including Denmark Hill;
- Blue daytime parking permit holders (allowing parking at PRUH and Orpington Hospital); and
- Green ‘off peak’ parking permit holders (15:45-09:00).

6.4.7 At present approximately 1,166 blue permits, 332 green permits and 246 gold permits have been issued to staff. PRUH implement a Car Parking Policy which is regularly reviewed. This policy ensures that permits are only allocated those with a business need, a complicated and long journey to work using public transport or have personal circumstances such as a disability or child care responsibilities for example. It is anticipated the uptake of staff parking permit would be increased as a result of this development, the Council highway officers advised that the details confirming the uptake of parking permit should be provide to the council upon six months of this first use of the parking deck. These details should be secured by a planning condition.

Disabled parking spaces

6.4.8 There are 29 disabled parking spaces within the wider hospital site. It is noted that there are no further designated disabled parking spaces. However, the blue badge holders can park in marked parking spaces for free when on site as patient or visitor. It is considered that the absence of designated disabled parking spaces to make more efficient use of the limited parking spaces is acceptable.

Electric vehicle charging points

6.4.8 Eighty electric vehicle charging points would be provided of which, forty would be active and the remaining would be passive. This provision would be above the Bromley Local Plan Policy requirement (1 in every 5 space) and it should be noted that there is no specific London Plan Policy requirement for non-residential development.

Cycle

6.5.5 The proposal would comply with London Plan Policy T5 which sets the standard for cycle parking. There are existing cycle storage areas and showering facilities at the hospital. Given that the proposal would not

introduce more hospital floor space or changes to the existing services/staff, the absence of the cycle parking facilities is justified in this instance.

Construction Management Plan

6.5.6 A construction management plan including construction vehicle routing, wheel washing, security and access controls, details of waste management and number of times of deliveries should be provided. These details should be provided and will be secured by a planning condition. The construction management plan should also include the full details of temporary staff car park arrangements and confirm its availability prior to any construction work is commenced on site. This provision should be maintained until the first use of the car park deck.

6.6 Trees - Acceptable

6.6.1 Trees play an important role within the urban environment. London Plan Policy G7 states development proposal should ensure that wherever possible, existing trees of value are retained. BLP Policy 73 states new development will be required to take particular account of existing trees on the site and on adjoining land, which in the interest of visual amenity and/or wildlife habitat.

6.6.2 There are no tree served with tree preservation order within the site. An Arboricultural Impact Assessment is submitted which that the following trees / planting in the surface level car park would be removed:

- G9 Sliver maple (category B) – 10 trees
- T10 Pine (category C) – 1 tree
- G11 Oak (category B) – 2 trees
- G12 Ash (category C) – 1 tree
- T13 Willow (category C) – 1 tree
- G14 Cyress (category C) – 5 trees
- G15 Birch (category B) – 3 trees
- T16 House Chestnut (category C) – 1 tree
- H17 Mixed species (category C) – 1 tree
- G18 Cypress (category C) – 1 tree
- T19 Typlip Tree (category C) – 1 tree
- G44 to G47 Ash (category C) – 30 trees
- G48 to G49 Robinia (category C) – 2 trees

6.6.3 The proposal would result in the loss of a number of low-quality trees in the hospital car park which cannot be immediate mitigated in the short-term. However, a landscaping plan is provided which indicated replacement planting would be provided adjacent to the proposed car park deck or within the hospital site. The Council's tree officer raised no objection to the proposal. Subject to the replacement planting, it is considered that the replacement trees would mitigate the loss of trees and planting with the hospital site.

6.7 Ecology - Acceptable

- 6.7.1 BLP Policy 72 states planning permission will not be granted for development that will have an adverse effect on protected species, unless mitigation measures can be secured to facilitate survival, reduce disturbance, or provide alternative habitats. London Plan Policy G6 states that development proposals should manage impacts on biodiversity and aim to secure net biodiversity gain.
- 6.7.2 The submitted preliminary ecological appraisal indicates that the habitats specified at the hospital site are considered to be of value within the immediate vicinity of the site only. The site is unlikely to support rare or protected species. As such, the impact on the habitats or potential roosting bats would be negligible and no surveys would be required.
- 6.7.3 The report recommends that vegetation clearance in the existing car park should be undertaken outside of the bird breeding season. If the development proceeds within the bird breeding season then the work should be carried out under the supervision of a qualified ecologist to avoid killing birds or destroying their nests. This would be secured by a planning condition.

Urban greening

- 6.7.4 London Plan Policy G5 states major development should contribute to the greening of London including urban greening as a fundamental element of site and building design, and by incorporating measures such as high-quality landscaping, green roofs, green wall and nature-based sustainable drainage. The London plan recommends a target score of 0.4 for developments that are predominantly residential and 0.3 for commercial development. London Plan Policy G6 states development should manage impacts on biodiversity and aim to secure net biodiversity gain.
- 6.7.5 The submitted information indicates that the proposal could achieve an Urban Greening Factor of 0.22 with new planting within hospital site. Given the limitation of the application site and there is no specific target for hospital, it is considered that proposal would be acceptable.

6.8 Drainage and flooding - Acceptable

- 6.8.1 The application site is located within Flood Zone 1 where the probability of river or sea flooding is less than 0.1% (1 in 1000) chance in any given year as defined by the Environment Agency. The Environment Agency mapping record indicates that the hospital is subjected to **low to high surface water flooding**.
- 6.8.2 The NPPF states that major development should incorporate sustainable drainage systems which should take account of advice from

the lead flood authority; have appropriate proposed minimum operational standards; have maintenance arrangements in place to ensure an acceptable standard of operation for the lifetime of the development; and where possible, provide multifunctional benefits.

6.8.3 Surface water flooding occurs when rainwater does not drain away through the drainage systems or soak into the ground. London Plan Policy SI-13 and BLP Policy 116 states development proposals should aim to achieve greenfield run-off rates and ensure that surface water run-off is managed as close to its source as possible. This is to ensure any surface water will be accommodated within the site and therefore prevent potential exceedance flows off-site.

6.8.4 At planning application stage, the following information are received:

- (1) existing drainage survey drawing;
- (2) CCTV drainage report;
- (3) historic record drawings;
- (4) Thames Water asset location search record; and,
- (5) Thames Water confirmation on no incidents of Sewer Flooding History onsite.

6.8.5 The Council is a lead local flood authority and has a responsibility to manage risk from surface water flooding. The Council highway drainage officer have advised that the drainage system is overwhelmed in the areas such as Crofton Road where reports of flooding were received on many occasions.

6.8.6 Underground attenuate surface water storage is one of the common mitigation measures to manage surface water flooding. Officers note that the proposed car deck is designed to ensure any temporary displacement of staff, patient and visitor parking spaces at the hospital site or any disruption to the daily hospital operation can be minimized. It would not be feasible to provide any underground mitigation measures at this car park site. This approach is fully supported by officers as it is essential to ensure the operation of the hospital can be sustained.

6.8.7 Surface water flooding should be mitigated at this source. The daily operation and activities associated to the hospital should also be free from risk of any form of flooding. The Council's highway drainage officer have advised that an overlay drawing to confirm the proposed site surface water drainage is connected into the existing drainage system and demonstrate the existing pump discharge rate of 4.361 liter/second can be maintained associated to the proposal should be provided. The Council's highway drainage officer has also advised that mitigation measures such as ponding, up to 150mm during big events to ease some pressure on the drainage network downstream should be considered. Should planning permission be recommended, it is considered that the above details and an achievable agreed measures

to ensure the existing facilities can accommodate the proposal should be secured by planning conditions.

6.9 Energy - Acceptable

6.9.1 BLP Policy 124 and London Plan Policy SI 2 requires new development should be net zero- carbon and an energy assessment in line with the GLA energy hierarchy (Be Lean, Be Clean, Be Green and Be Seen measures) should be provided.

6.9.2 The proposed car park deck would be an open-air structure and no heating would be required as part of this proposal. Due to its unique nature of the proposal, it is not considered to be feasible to install on-site renewable energy measures for a standalone car park deck. Given that there are existing external lightings already exists in the existing surface level car park, it is on balance considered that the absence of energy measures for an open-air car park deck would not be unacceptable in this instance.

6.10 Other matters

- London and Local CIL

6.11 This development is not liable to the Mayor of London's CIL and Local CIL.

- Head of Terms

6.12 None required.

7.0 Conclusion

7.1 The proposal to introduce a 197 spaces car park deck is designed to meet the unmet provision and to ensure further improvement of hospital services can be sustained at this hospital site. It is note that the provision of additional parking spaces would not comply with the London Plan. However, it should be noted that the proposal would result in a net increase of 36 parking spaces and is not considered disproportionate at this location with a moderate accessibility rating by public transport.

7.2 The siting, layout and design of the proposal is considered to be acceptable and would not have an adverse impact on the residential amenities to the neighbouring properties. Replacement planting, surface water drainage details and appropriate measures will be secured by planning conditions.

7.3 The proposal would result in a temporary closure of a surface level park with 195 spaces. An off-site staff car park with 130 parking spaces parking will be provided to mitigate its impact during the construction of the car park deck. A staff shuttle bus will also be provided. As such, it is

considered that any disruption as a result of this proposal will be limited to a temporary basis and adequate mitigation will be provided during the construction period.

- 7.4 Subject to the planning conditions, it is considered that the proposal would be acceptable in planning terms and is recommended planning permission be granted.

RECOMMENDATION

Planning permission be granted, subject to the following planning conditions and any other conditions may be requested by Assistant Director.

SUMMARY OF CONDITIONS AND INFORMATIVES

Conditions

1. Time period
2. Approved plan
3. Construction and Environmental Management Plan including replacement staff parking arrangement
4. Surface water drainage calculations and mitigation measures
5. Details of staff parking permits, upon the six months of the first use of the car park deck.

Compliance

6. The agreed off-site staff parking arrangement shall remain in place prior to commencement of the development until the first use of the car park deck.
7. Replacement planting
8. Comply with the lighting assessment
9. EVCP
10. Report of any unexpected contamination
11. Fire statement
12. Preliminary ecology report
13. Landscaping details
14. Secured by design

Informative

- Thames Water
- Report of any unexpected contamination